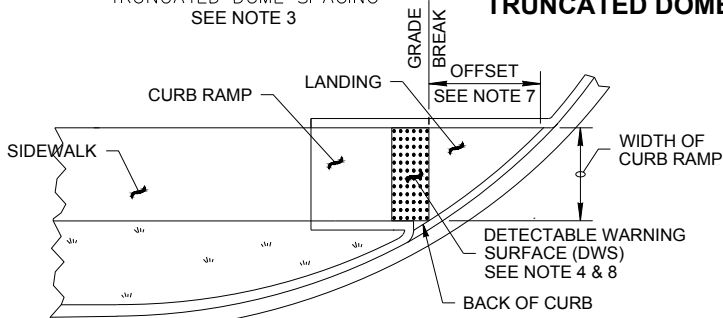


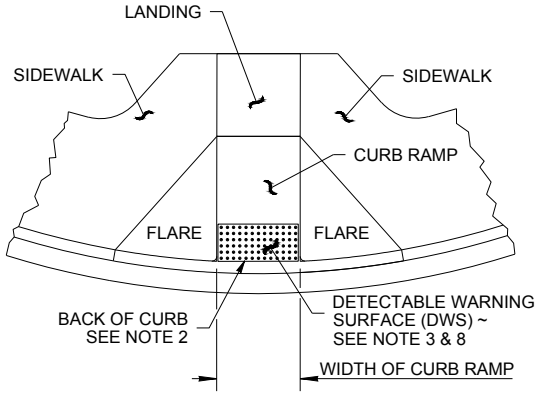
SEE STANDARD SPECIFICATIONS FOR COLOR OF SURFACE

TRUNCATED DOME SPACING
SEE NOTE 3

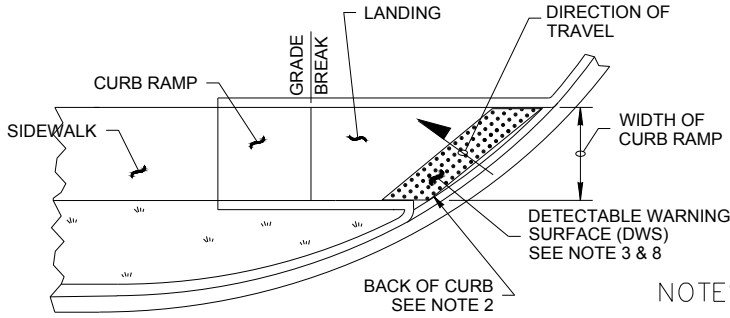
TRUNCATED DOME DETAILS



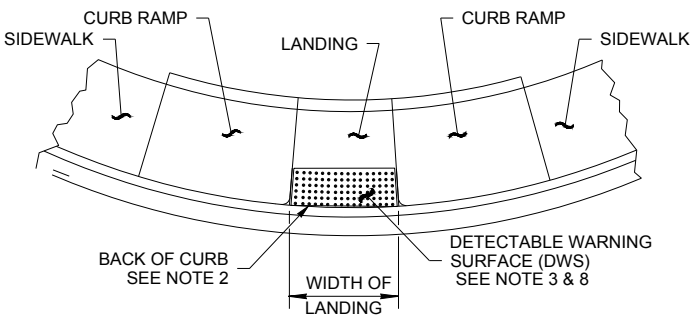
SINGLE DIRECTION CURB RAMP
(BOTH ENDS OF THE BOTTOM GRADE ARE \leq 5 FT. FROM BACK OF CURB)
(SEE NOTE 7)



PERPENDICULAR CURB RAMP
(SEE NOTE 6)



SINGLE DIRECTION CURB RAMP
(ONE OR BOTH ENDS OF THE BOTTOM GRADE BREAK ARE $>$ 5 FT. FROM BACK OF CURB)
(SEE NOTE 7)



PARALLEL CURB RAMP
(SEE NOTE 6)

NOTES

1. The Detectable Warning Surface (DWS) shall extend the full width of the curb ramp (exclusive of flares) or the landing.
2. The Detectable Warning Surface (DWS) shall be placed at the back of the curb, with the two leading corners of the DWS panel placed adjacent to the back of the curb, and with no more than a 2 inch gap between the DWS and the back of the curb measured at the center of the DWS panel.
3. The rows of truncated domes shall be aligned to be perpendicular to the grade break at the back of curb.
4. The rows of truncated domes shall be aligned to be parallel to the direction of travel.
5. If curb and gutter are not present, such as a shared-use path connection, the Detectable Warning Surface shall be placed at the pavement edge.
6. See **CLARK COUNTY STANDARD DETAILS** for sidewalk and curb ramp details.
7. When the grade break between the curb ramp and the landing is less than or equal to 5 ft. from the back of curb at all points, place the Detectable Warning Surface on the bottom of the curb ramp.
8. Detectable Warning Surface shall extend a minimum 2' in the direction of pedestrian travel.

NO.	REVISIONS	DATE	BY
1	Added Note #8	2/10/16	BK
2	Modify Notes	5/1/17	bk

DWG: F10.DWG



Department of Public Works
CLARK COUNTY WASHINGTON
proud past, promising future

DETECTABLE WARNING SURFACE
APPROVED
COUNTY ENGINEER

STANDARD
F10
DETAIL
DESIGNED
DRAWN
DATE 01/26/16

01/26/16
DATE